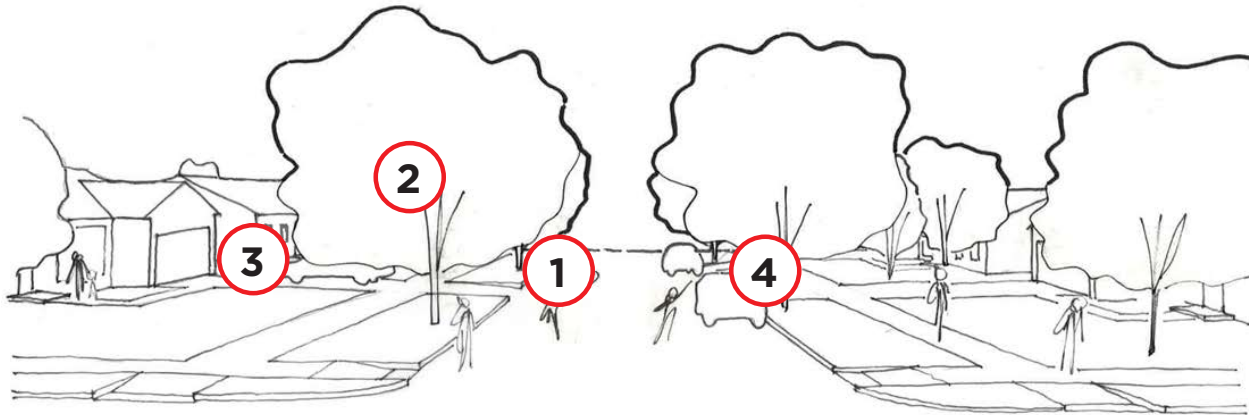


## NEIGHBORHOOD RESIDENTIAL



1. Narrow travel ways, slow speeds
2. Abundant green space
3. Driveways are common
4. On-street parking

**Neighborhood Residential** streets invite residents to use the streets as common gathering places and linear green space.

Much of Grand Rapids consists of quiet residential communities characterized as “Modern,” “Mid-Century” or “Traditional” in the City’s Neighborhood Pattern Book.

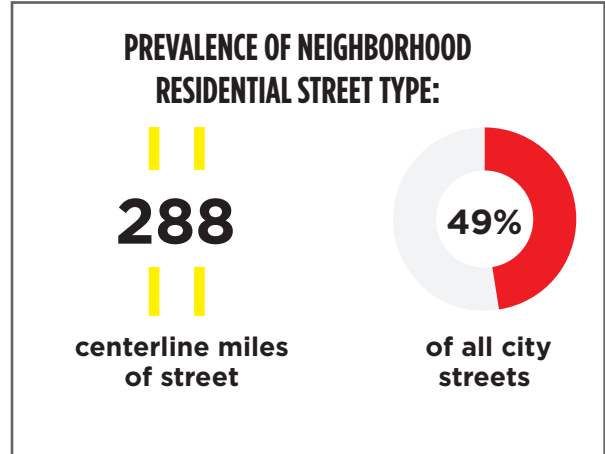
Land use along these streets is primarily low-to-moderate density in nature, generally with single-family detached or duplex style homes.

Neighborhood Residential streets are not principal streets in the regional vehicular transportation network, although they may serve as an important link for pedestrians and community bicyclists who generally travel at lower speeds. Neighborhood Residential streets generally do not feature transit service. Additionally, truck traffic may be restricted on these streets.

These streets typically have limited connectivity to the larger network.

### EXAMPLE STREETS:

- » Oakleigh Avenue, Westend Avenue, or Jackson Street NW.
- » Lamberton, Graceland, or Mason Streets NE.



- » Morris Avenue, Griswold Street, or Meadowbrook Street SE.
- » Dayton Street, Caulfield Avenue, or Olympia Street SE.

### ANTICIPATED AND DESIRED USES:

- » Community interactions in the public rights-of-way such as visiting neighbors.
- » Shared use of the street as an extension of recreational space.
- » Community low-stress non-motorized travel by foot or bicycle.
- » Property access through driveways and on-street parking.
- » Short distance (last block) vehicle travel.
- » Grey and green infrastructure (utility corridors).

## PRIORITY USERS:

- » **Pedestrians** of all type and abilities but with particular attention to **vulnerable users** including young children, seniors, and persons with disabilities.

## DESIGN OBJECTIVES:

- » Maintain low vehicle volumes and low travel speeds.
- » Design streets as linear greenways and open spaces.
- » Provide access to homes and residences.

## TYPICAL DESIGN FEATURES AND TREATMENTS:

- » Narrow street travel ways that are most commonly bi-directional “yield” streets without a marked center line. Vehicles must pull to the side and slow or stop to enable an approaching vehicle to pass.
- » The limited connectivity and narrow travel lanes generally manage speeds and deter non-local traffic, however in some cases active speed control or traffic deterrents may be needed.
- » Intersections may be stop-controlled, yield-controlled or uncontrolled. Intersections may have diverters, neck downs, or other traffic calming treatments.
- » Crosswalks generally are not marked. Pedestrians may comfortably cross the street at any point along its length.
- » Properties may be accessed from the street or by way of alleys. Multiple access points and driveways are common.

- » Adequate front yards and parkways to support large street trees and dense canopies.
- » Sidewalks on both sides of the street are preferred unless the street is a shared street. A shared street is a street where pedestrians and other users share and mix together using the entire streetscape. Shared streets are typically single block streets with limited connectivity and very few vehicles.
- » Impervious surfaces are minimized.
- » On-street parking is generally provided on one or both sides of the street.
- » Separate bicycle facilities are generally not provided. Vehicle speeds and volumes are low enough to provide safe and low-stress bicycle accommodation within the street.
- » For streets included in the low stress bicycle network, traffic calming, diverters, wayfinding, and other treatments should be provided.

## TYPICAL/TARGET METRICS:

- » Vehicle volumes below 2,000 vehicles per day
- » Vehicle speeds <25 MPH
- » Face-to-face of curb 26' to 30'

