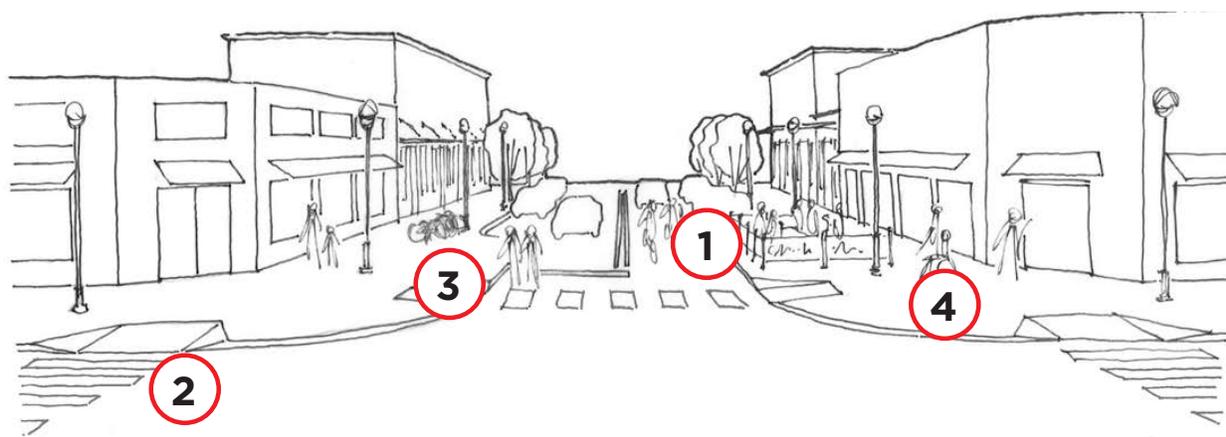


NEIGHBORHOOD BUSINESS



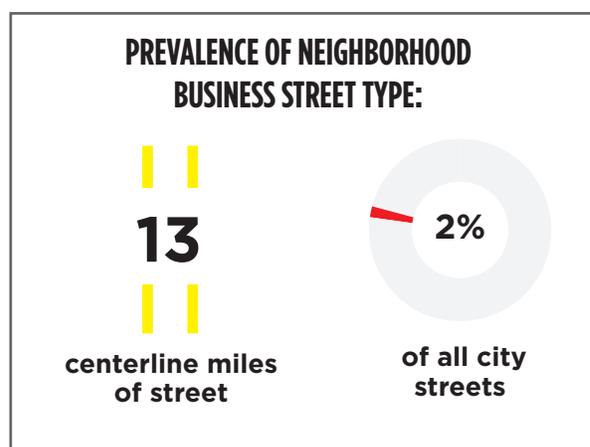
1. Narrow travel lanes
2. Frequent pedestrian crossings
3. Parking for both vehicles and bicycles
4. Generous sidewalks

The **Neighborhood Business** streets are typically compact areas that occupy only a segment along a longer street corridor. Neighborhood business districts are unique areas within Grand Rapids neighborhoods. They provide dining, shopping and employment opportunities while adding character and commerce to the Grand Rapids economy. Neighborhood Business districts are not just destinations for local residents, they are visited by patrons from across the region.

Neighborhood Business streets are generally moderate to higher volume multimodal streets.

The quality of the pedestrian environment is of paramount importance. Inviting sidewalk zones correspond positively with higher retail sales and greater commercial value of properties along Neighborhood Business streets. Pedestrians must be able to cross the street safely at multiple points to access the many offerings of the district. The pedestrian zone is buffered from roadway traffic by curbside parking or a generous amenity zone to increase pedestrian comfort. Formal or informal seating is common to invite pedestrians to gather, visit and linger along the street. Pedestrian scaled street lighting increases the attractiveness of the street during evening hours.

Access for delivery vehicles, patrons and workers is equally critical to the success



of Neighborhood Business streets. These users may arrive by foot, bicycle, transit, or personal vehicle, and all modes should be comfortably accommodated.

Street vehicle speeds should be slow and well managed.

Robust tree canopies contribute positively to the economic productivity of these streets and districts. Studies have shown that patrons will stay longer and spend more on tree-lined streets compared to those bereft of tree coverage.

Neighborhood Business streets are critical segments in the larger city or regional street network. They accommodate travel demands both to and through the business district and must provide safe access and mobility for all modes of travel, although they may be prioritized for one or more modal emphases. It is often difficult to provide separated or protected bicycle facilities on these streets given the variety and volume of mobility demands. However, when the segment is a component of the bicycle priority network, designated and marked facilities must be accommodated.

EXAMPLE STREETS:

- » Wealthy Street SE from Union to Auburn
- » Michigan Street NE from Prospect to Fuller
- » Grandville Ave SW from Hall to Grant

ANTICIPATED AND DESIRED USES:

- » Commercial activities such as café dining or outdoor retail.
- » Residential uses both at the ground and upper levels.
- » Patron, client, and employee access by way of vehicle and bicycle parking, quality transit stops, and inviting pedestrian zones.
- » Moderate-to-high pedestrian volumes.
- » Moderate-to-high frequency bus transit service and access.
- » Delivery truck access.

PRIORITY USERS:

- » **Commercial patrons and visitors.**
- » Delivery vehicles.
- » Workers and proprietors.

DESIGN OBJECTIVES:

- Support and strengthen economic productivity and value.
- Enable efficient and unobtrusive delivery of goods and/or high frequency and brief commercial transactions (e.g. short duration stops).
- Enhance street quality and image.
- Enhance access via all modes (pedestrian, bicycle, transit, personal vehicle).
- Accommodate multimodal through travel.

TYPICAL DESIGN FEATURES AND TREATMENTS

- » Narrow travel lanes to slow traffic speeds and minimize pedestrian crossing distance.
- » Bi-directional street operations are preferred. Center line may or may not be marked.
- » Short block lengths and frequent intersections are preferred. Intersections may be stop or signal controlled, or uncontrolled.
- » Pedestrian crossings along the length of the segment should be anticipated.

Crosswalks may or may not be marked at uncontrolled locations. Midblock crossings connect trip generators on opposing sides of the street and minimize out of direction travel for pedestrians. Crosswalks should be marked at controlled intersections.

- » Streets must provide generous sidewalks on both sides, adequately buffered from vehicle traffic. Pedestrian seating is recommended.
- » On-street parking on one or both sides of the street is preferred. Sufficient and convenient bicycle parking is required.
- » Parking should be well managed to optimize occupancy while concurrently providing a limited but continuous amount of available access. Parking may or may not be metered.
- » Adequately scaled and spaced loading zones are required to support commercial needs. Loading periods may be managed and loading zone usage enforced.
- » Transit service is common and encouraged on Neighborhood Business streets. Transit stops should provide adequate amenities for a quality rider experience. Transit amenities must not constrain the minimum required pedestrian clear zone.
- » Curb cuts and driveways should be discouraged and minimized in favor of alleys and shared access points from minor and intersecting streets.
- » Large canopy trees are desired. Creativity is encouraged to promote green infrastructure.
- » Streetscape should provide a quality environment. Standard materials, installed with quality workmanship, are acceptable and in many cases encouraged. Special materials may be used if maintenance agreements are provided.
- » Public art, wayfinding, and other unique features of place are appropriate and encouraged.

TYPICAL/TARGET METRICS

- » Vehicle volumes greater than 5,000 vehicles per day
- » Vehicle speeds <25 MPH
- » Face-to-face of curb 36' to 58'